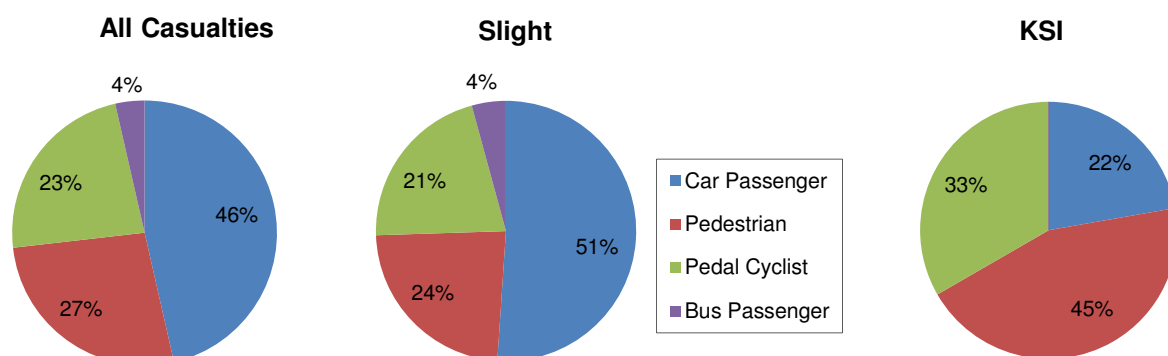


	Bolsover District	County
Child casualties as a % of all casualties	9%	7%
KSI child casualties as a % of all KSI casualties	12%	6%
Child casualties per 1000 population	6.6	6.8
Pedestrians per 1000 population	1.8	2.3
Pedal Cyclists per 1000 population	1.5	0.9
Car Passengers per 1000 population	3.1	3.1
0-4 year old casualties per 1000	2.5	2.7
5-10 year old casualties per 1000	7.2	6.7
11-15 year old casualties per 1000	10.1	10.3



Notes:

- Pedal cyclist collisions have increased since the last study. A higher proportion of casualties were pedal cyclists than the County average
- Fewer pedestrians were injured than the County average
- A similar percentage of car passengers were injured to the County average
- The number of primary aged casualties has increased since the last study
- Boys were more likely to be injured than girls
- February and September were the months with most casualties
- Over 80% of casualties were injured during the week, higher than the County average
- There were peaks in casualties between 3pm and 6pm
- Just 12% of car passengers were travelling with a young driver, lower than the County average

Where casualty's postcode is recorded:

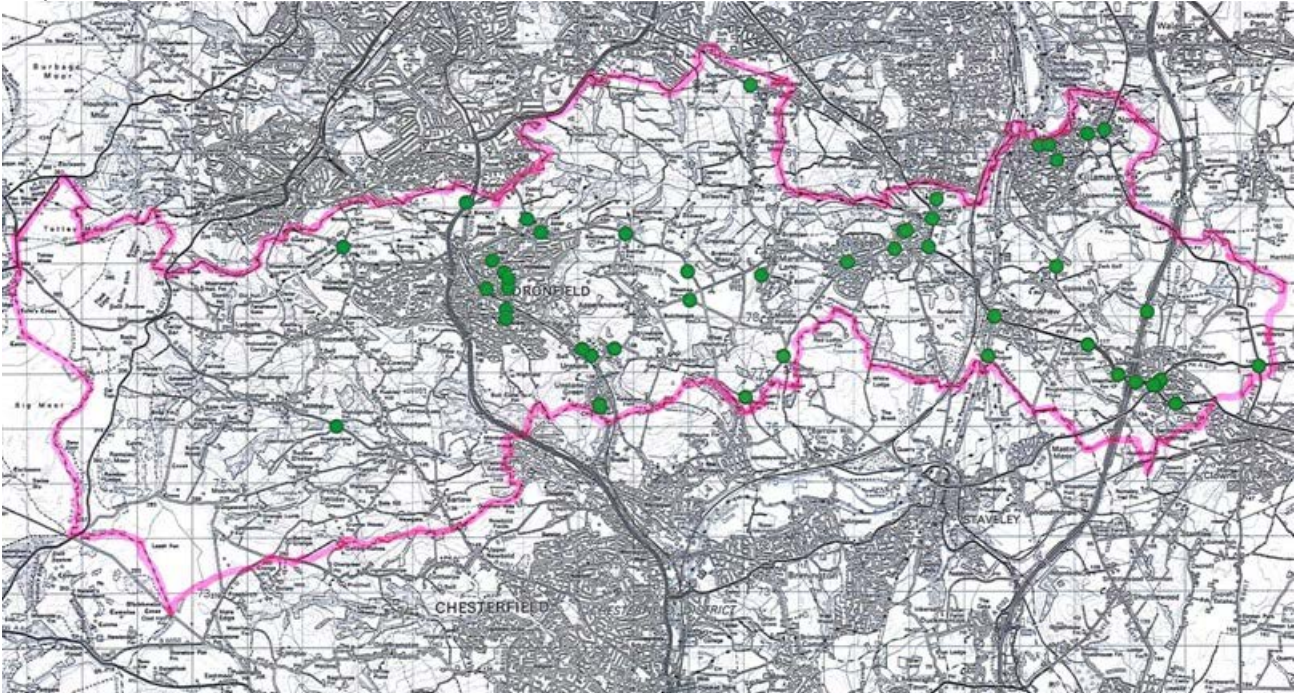
- All pedestrians lived within a mile of where they were injured
- All but one pedal cyclist lived within a mile of where they were injured and all lived within 5 miles
- Nearly three quarters of car passenger lived within 5 miles of where they were injured and all lived within 11 miles

Areas to look at:

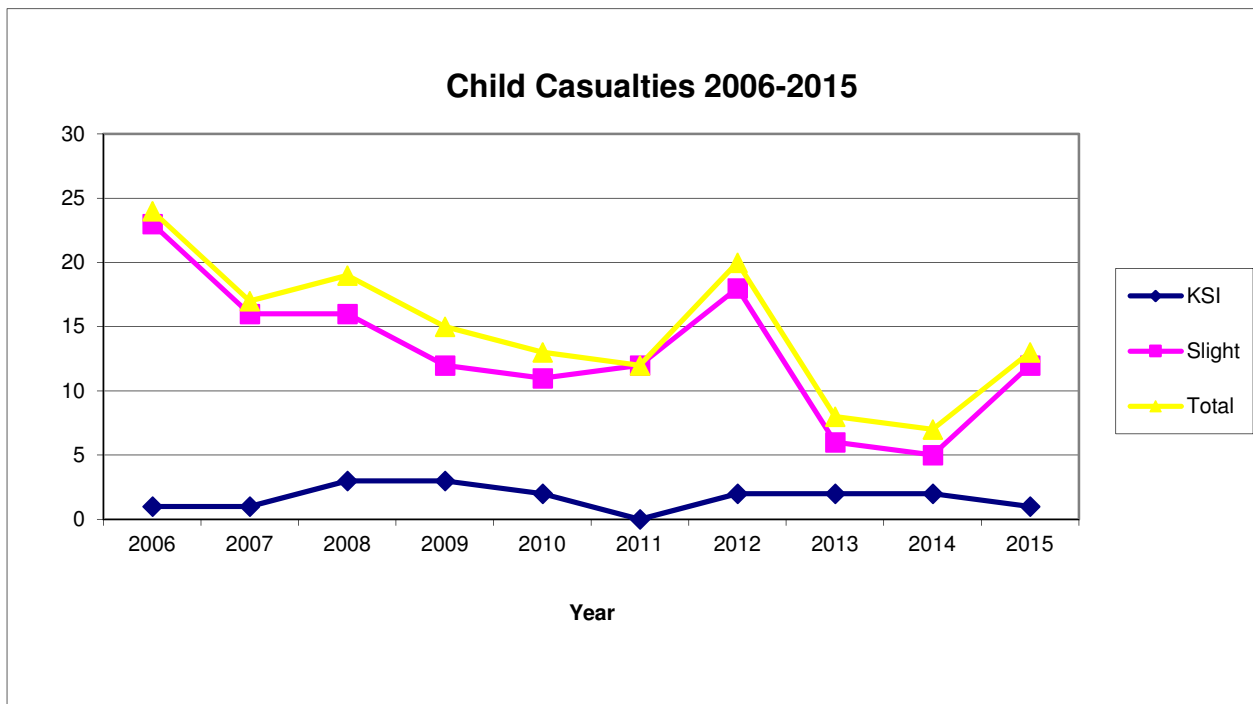
- Pedal cyclists – particularly 7-13 year old boys
- 0-7 year old car passengers
- 11-13 year old pedestrians

Dronfield, Killamarsh & District

2 secondary schools
24 primary schools

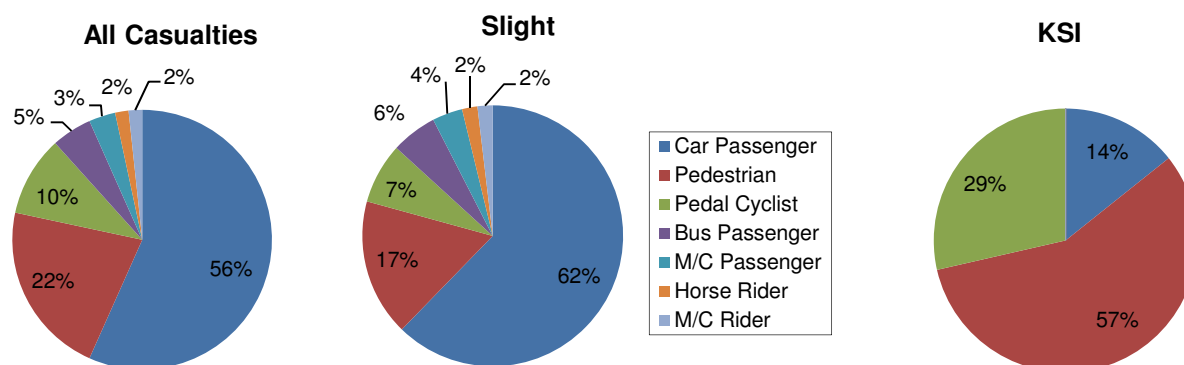


7 KSI casualties 2011-2015
60 Slight casualties 2011-2015



24% decrease in 2011-15 from 2008-2012, better than the County average

	Dronfield District	County
Child casualties as a % of all casualties	9%	7%
KSI child casualties as a % of all KSI casualties	8%	6%
Child casualties per 1000 population	7.2	6.8
Pedestrians per 1000 population	1.6	2.3
Pedal Cyclists per 1000 population	0.7	0.9
Car Passengers per 1000 population	4.1	3.1
0-4 year old casualties per 1000	3.8	2.7
5-10 year old casualties per 1000	4.6	6.7
11-15 year old casualties per 1000	12.7	10.3



Notes:

- More car passengers were injured than the County average
- A lot less pedestrians were injured than the County average
- Less pedal cyclists were injured than the County average, though more were seriously injured
- 62% of child casualties were of secondary school age, higher than the figure for the County.
- Boys and girls were equally likely to be injured
- June and September were the months with most casualties
- 40% of casualties were injured at the weekend, much higher than the County average. Pedestrians were least likely to be injured at the weekend
- There were peaks in casualties between 3pm and 7pm
- There were less collisions in hours of darkness than the County average

Where casualty's postcode is recorded:

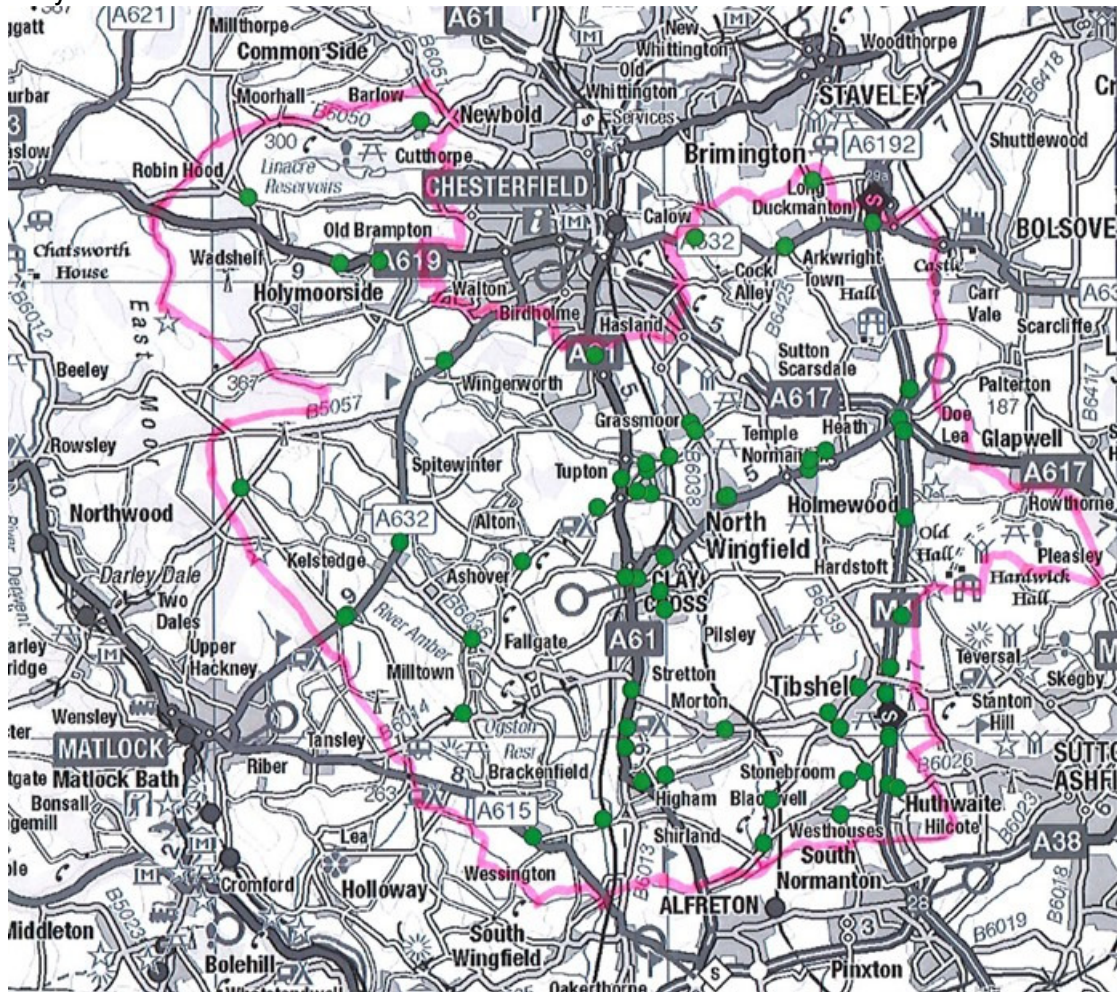
- 80% of pedestrians lived within 1 mile of where they were injured and all lived within 2 miles
- All pedal cyclists lived within 2 miles of where they were injured
- Two thirds of car passengers lived within 5 miles of where they were injured. Only 1 lived more than 12 miles from where they were injured

Areas to look at:

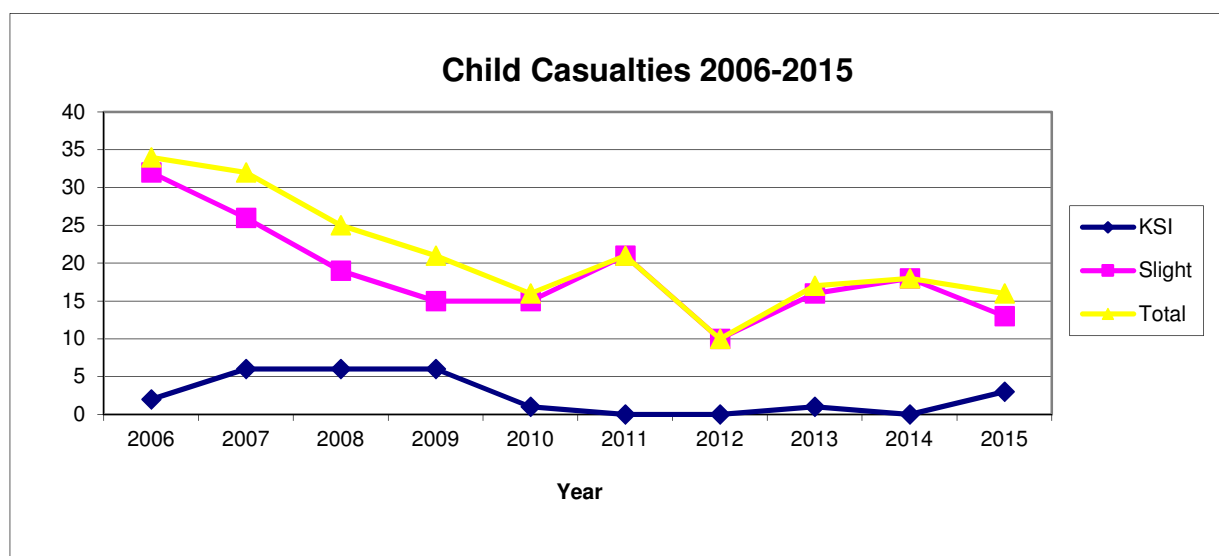
- 0-4 year old car passengers
- 8-15 year old car passengers
- 8-13 year old pedestrians
- 11-13 year old pedal cyclists

Tibshelf, Clay Cross & District

2 secondary schools
29 primary schools

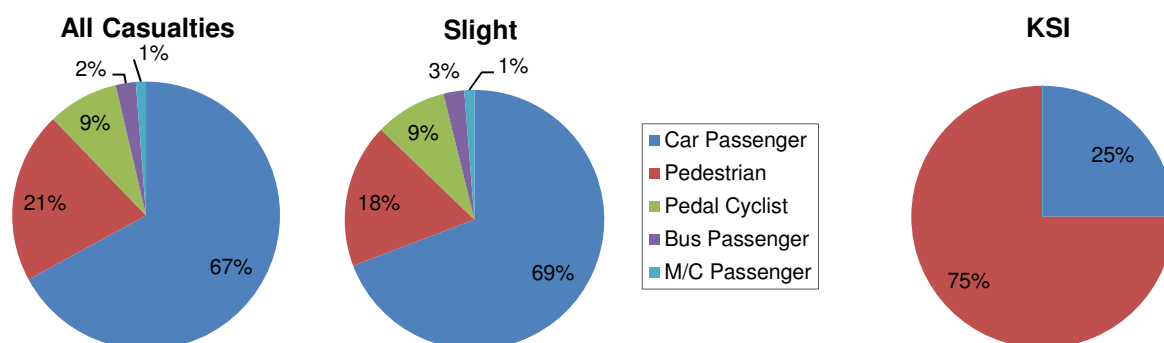


4 KSI casualties 2011-2015
78 Slight casualties 2011-2015



12% decrease in 2011-15 from 2008-2012, slower than the County average

	Tibshelf District	County
Child casualties as a % of all casualties	6%	7%
KSI child casualties as a % of all KSI casualties	2%	6%
Child casualties per 1000 population	8.0	6.8
Pedestrians per 1000 population	1.7	2.3
Pedal Cyclists per 1000 population	0.7	0.9
Car Passengers per 1000 population	5.4	3.1
0-4 year old casualties per 1000	2.6	2.7
5-10 year old casualties per 1000	9.1	6.7
11-15 year old casualties per 1000	11.8	10.3



Notes:

- A lot more car passengers were injured than the County average
- A third of car passengers were travelling with a young driver, much higher than the County average
- A third of car passenger casualties were injured on the motorway
- A lot less pedestrians were injured than the County average
- Less pedal cyclists were injured than the County average
- Half of child casualties were of secondary school age, similar to the figure for the County
- Girls and boys were equally likely to be injured but over 80% of 14 and 15 year olds injured were girls
- April and July were the months with most casualties
- Over a third of casualties were injured at the weekend, higher than the County average
- There were peaks in casualties between 12 noon and 1pm and 3pm and 6pm
- Teenagers were more likely to be injured during hours of darkness than the County average

Where casualty's postcode is recorded:

- All but 2 pedestrians lived within 1 mile of where they were injured and all lived within 3 miles
- All but one pedal cyclist lived within 1 mile of where they were injured
- A third of car passengers lived within 5 miles of where they were injured
- Excluding the motorway, half of car passengers lived within 5 miles of where they were injured and all lived less than 10 miles away

Areas to look at:

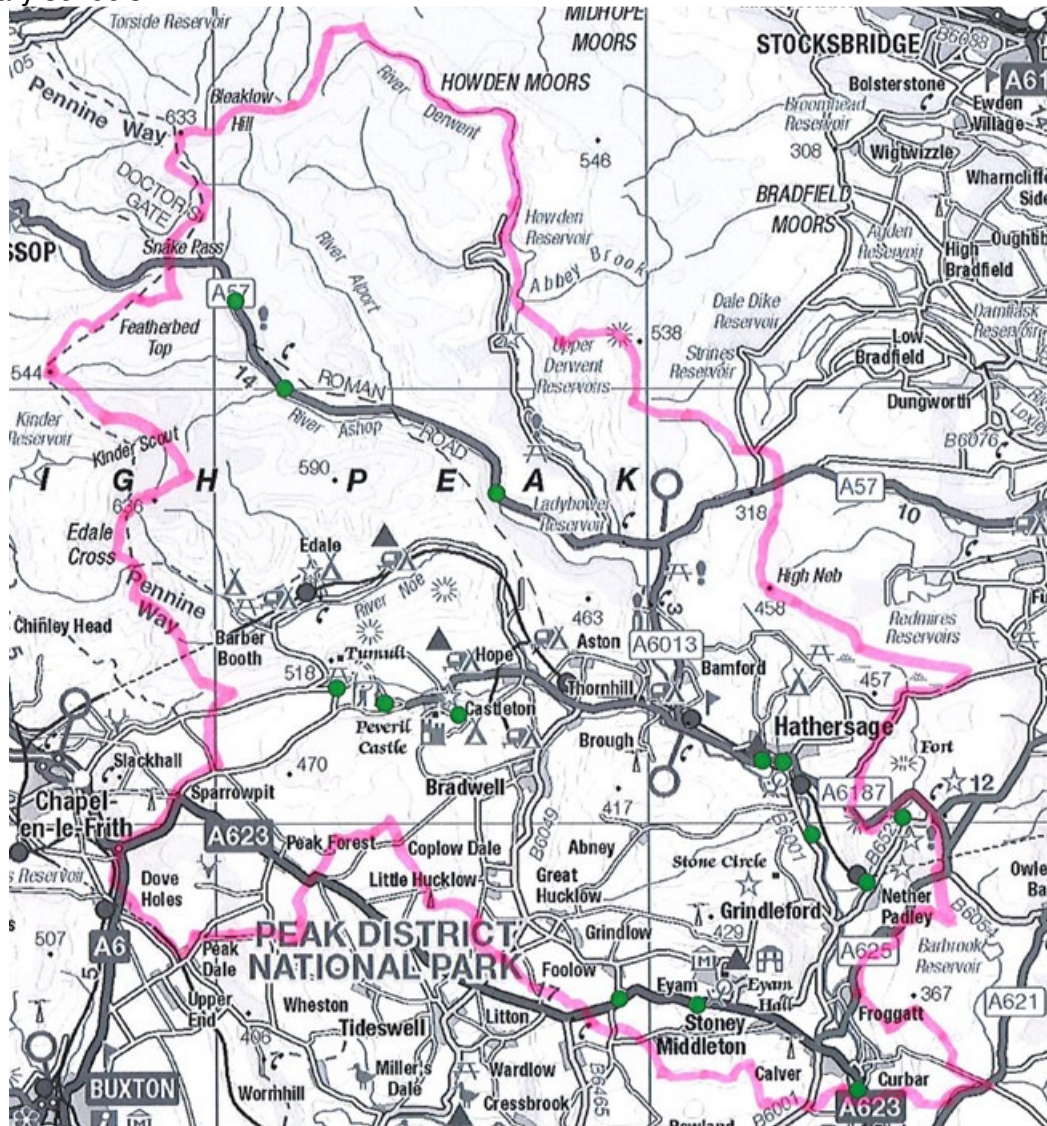
- Car passengers of all ages
- 11-13 year old pedestrians
- Car passengers travelling with young drivers, particularly 13 – 15 year old girls travelling with young men

Hathersage & Hope Valley District

Casualty numbers in this area were very low

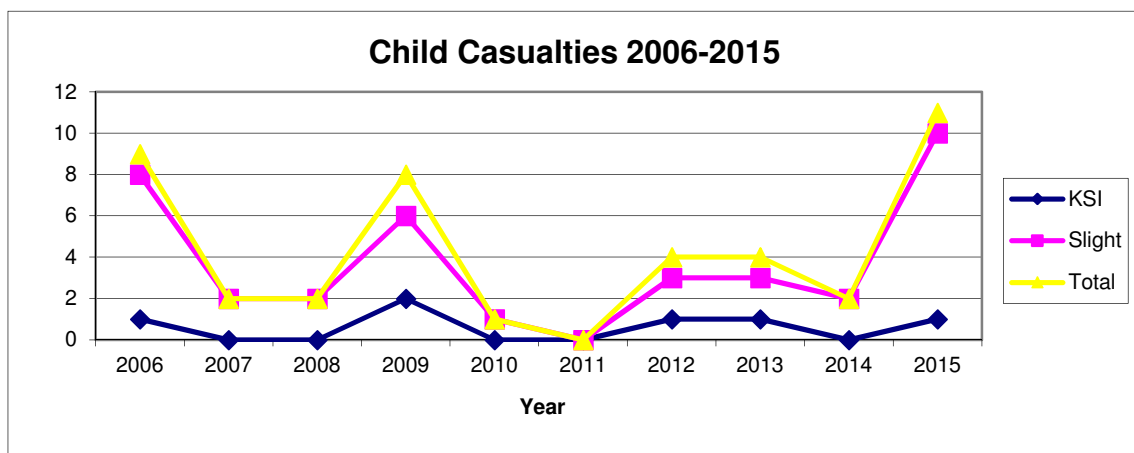
1 secondary school

13 primary schools



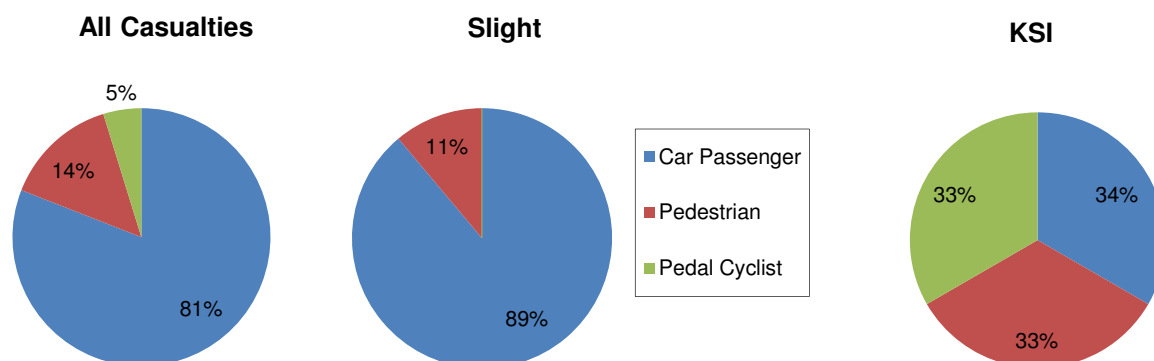
3 KSI casualties 201-2015

18 Slight casualties 2011-2015



40% increase in 2011-15 from 2008-2012, much worse than the County average

	Hathersage District	County
Child casualties as a % of all casualties	6%	7%
KSI child casualties as a % of all KSI casualties	3%	6%
Child casualties per 1000 population	11.7	6.8
Pedestrians per 1000 population	1.7	2.3
Pedal Cyclists per 1000 population	0.6	0.9
Car Passengers per 1000 population	9.4	3.1
0-4 year old casualties per 1000	10.6	2.7
5-10 year old casualties per 1000	11.3	6.7
11-15 year old casualties per 1000	12.8	10.3



Notes:

- Less pedestrians were injured than the County average
- Less pedal cyclists were injured than the County average
- More car passengers were injured than the County average
- Less car passengers were being driven by young drivers than the County average
- A higher percentage of casualties were pre-school age than the County average
- Girls were more likely to be injured than boys
- July and October were the months with most casualties
- Three quarters of casualties were injured during the week, similar to the County average
- There were no particular peaks in casualties at different times of the day

Where casualty's postcode is recorded:

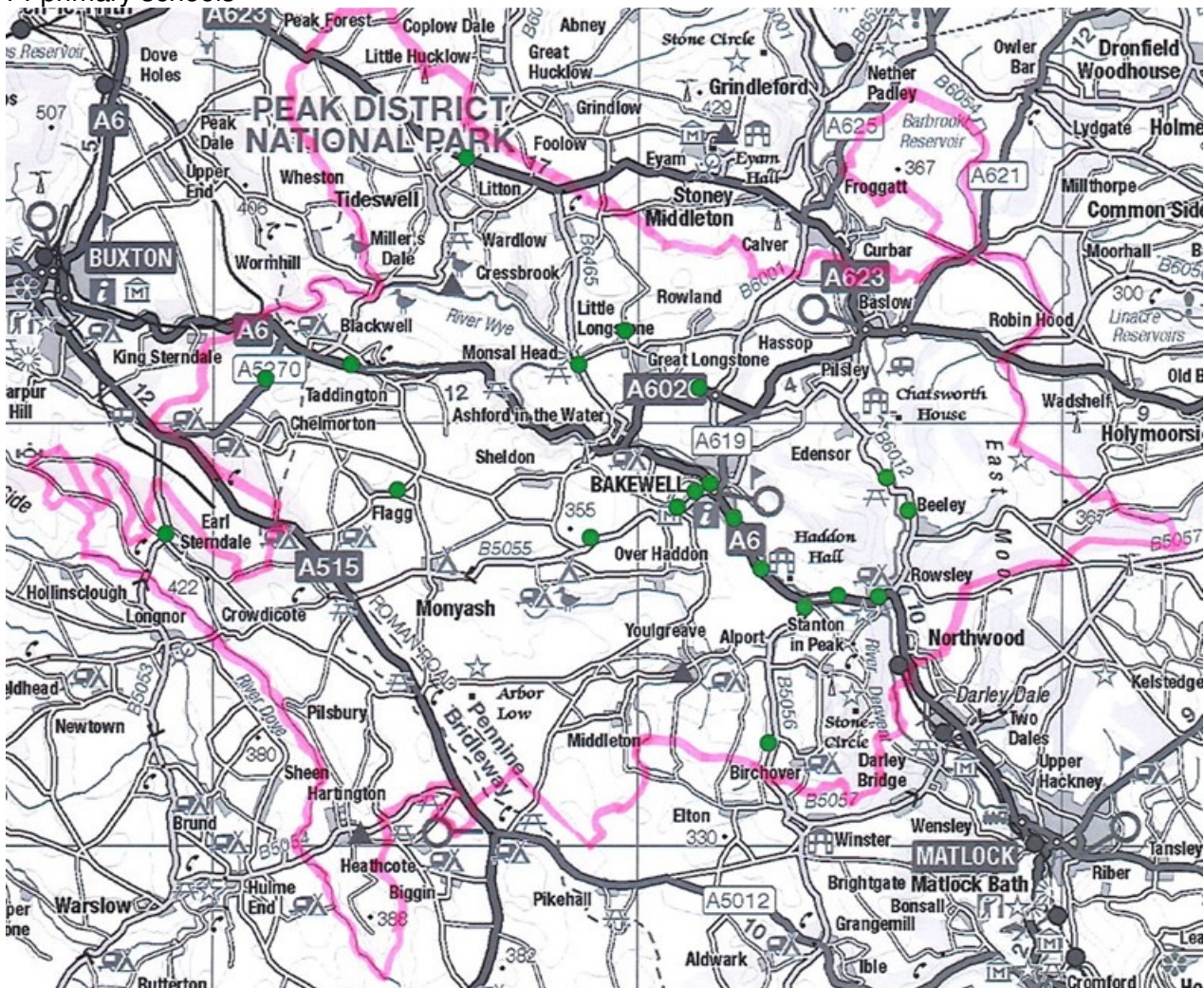
- Only 2 pedestrians had their postcode recorded. Both lived out of this area (35 and 100 miles away)
- The one pedal cyclist casualty lived 5 miles from where they were injured
- Only a quarter of car passengers lived within 5 miles of where they were injured. The others lived an average of 30 miles away, particularly in South Yorkshire and Greater Manchester

Areas to look at:

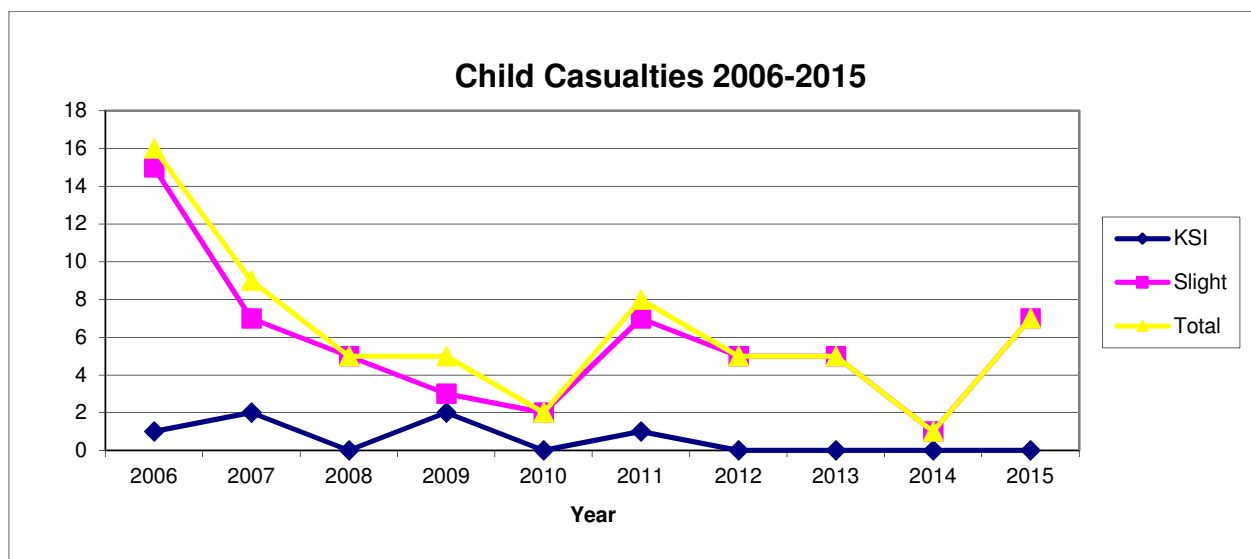
- Car passengers, particularly 0-4 year olds and 14-15 year olds
- Car passengers travelling into the area from further afield - tourists

Bakewell & District

1 secondary school
14 primary schools

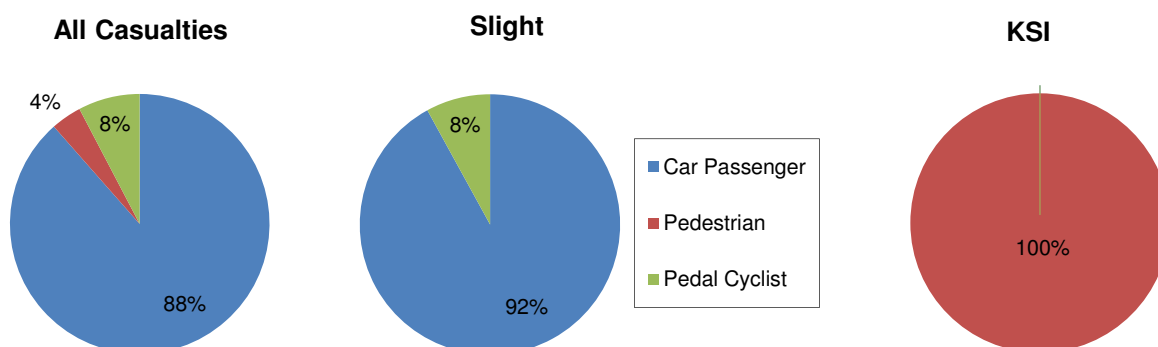


1 KSI casualty 2011-2015
25 Slight casualties 2011-2015



4% increase in 2011-15 from 2008-2012, worse than the County average

	Bakewell District	County
Child casualties as a % of all casualties	5%	7%
KSI child casualties as a % of all KSI casualties	1%	6%
Child casualties per 1000 population	10.3	6.8
Pedestrians per 1000 population	0.4	2.3
Pedal Cyclists per 1000 population	0.8	0.9
Car Passengers per 1000 population	9.1	3.1
0-4 year old casualties per 1000	3.0	2.7
5-10 year old casualties per 1000	8.7	6.7
11-15 year old casualties per 1000	17.1	10.3



Notes:

- More car passengers were injured than the County average
- Less pedestrians were injured than the County average, only one casualty was a pedestrian
- Less pedal cyclists were injured than the County average
- A higher proportion of secondary aged children were injured than the County average
- Nearly three quarters of the casualties were girls, higher than the County average
- July was the month with most casualties
- More than half of casualties were injured at the weekend, much higher than the County average
- There were peaks in collisions between 10 and 11am and 5 and 8pm
- A similar number of car passengers were being driven by young drivers to the County average

Where casualty's postcode is recorded:

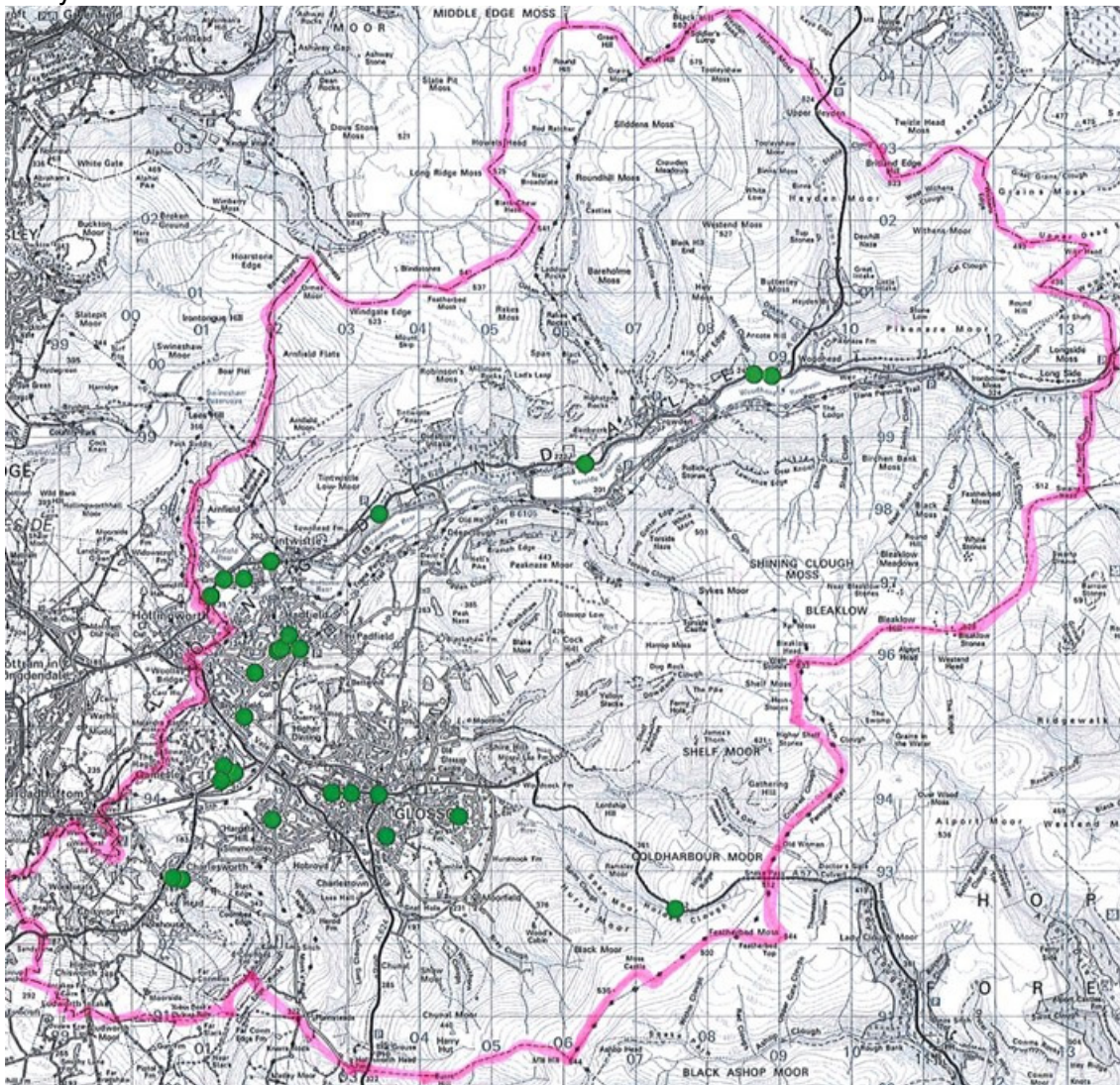
- The one pedestrian lived 15 miles from where they were injured
- One pedal cyclist had a postcode recorded. They lived less than 2 miles from where they were injured
- 40% of car passengers lived within 5 miles of where they were injured
- A quarter of car passengers lived over 20 miles of where they were injured

Areas to look at:

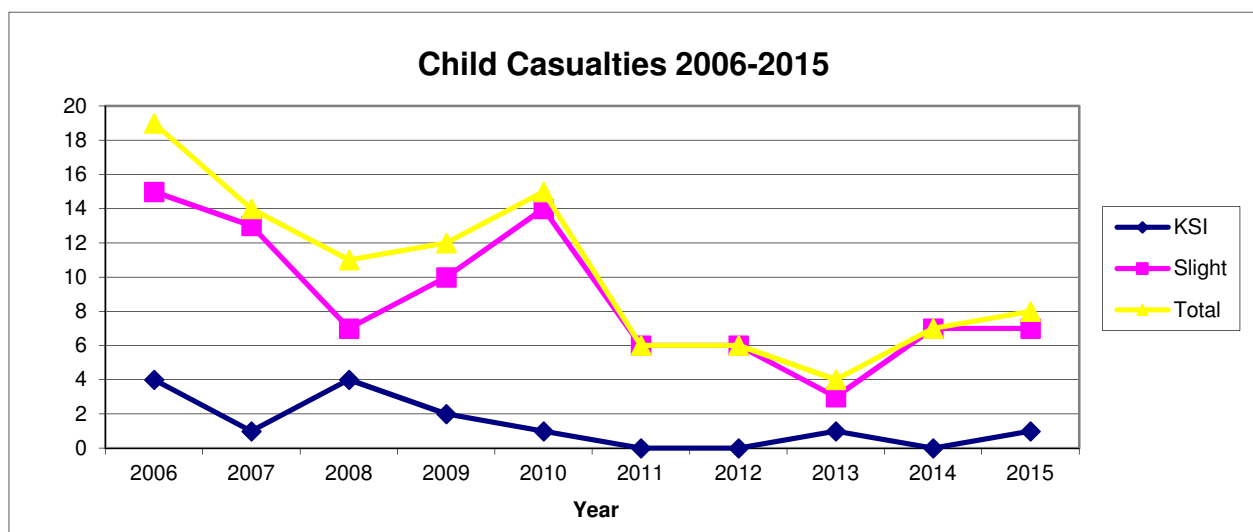
- Car passengers, particularly 14-15 year old girls

Glossop & District

2 secondary schools
15 primary schools

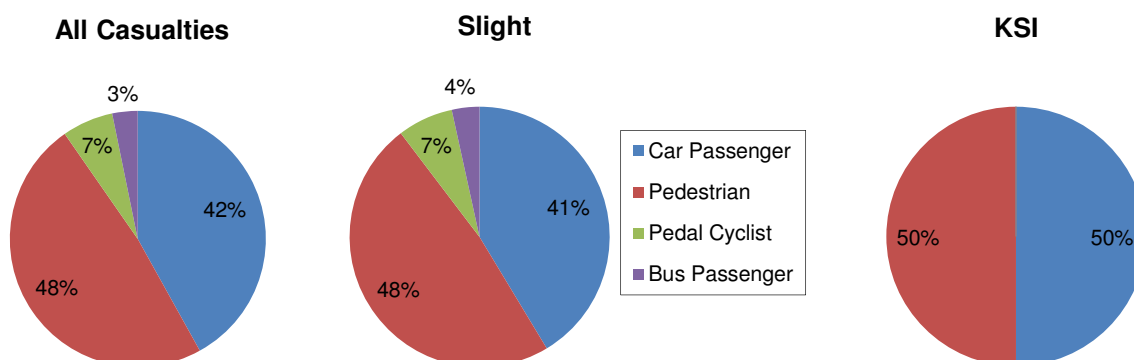


2 KSI casualties 2011-2015
31 Slight casualties 2011-2015



38% decrease in 2011-15 from 2008-2012, better than the County average

	Glossop District	County
Child casualties as a % of all casualties	7%	7%
KSI child casualties as a % of all KSI casualties	3%	6%
Child casualties per 1000 population	4.8	6.8
Pedestrians per 1000 population	2.3	2.3
Pedal Cyclists per 1000 population	0.3	0.9
Car Passengers per 1000 population	2.0	3.1
0-4 year old casualties per 1000	0.5	2.7
5-10 year old casualties per 1000	4.9	6.7
11-15 year old casualties per 1000	8.8	10.3



Notes:

- A slightly lower proportion of car passengers were injured than the County average
- A higher proportion of pedestrians were injured than the County average
- A lower proportion of pedal cyclists were injured than the County average
- Over 60% of casualties were secondary school age, higher than the figure for the County
- Boys and girls were equally likely to be injured
- September and December were the months with most casualties
- Nearly three quarters of casualties were injured during the week, similar to the County average
- There was a peak in casualties between 4pm and 6pm
- Over a third of car passengers were being driven by young drivers, higher than the County average

Where casualty's postcode is recorded:

- All but 2 pedestrians lived less than 2 miles from where they were injured. Only 1 pedestrian lived more than 5 miles away
- Only one pedal cyclist had their postcode recorded. They lived less than 1 miles from where they were injured
- Half of car passengers were injured less than 5 miles from where they lived
- Over 40% of car passengers lived over 20 miles from where they were injured

Areas to look at:

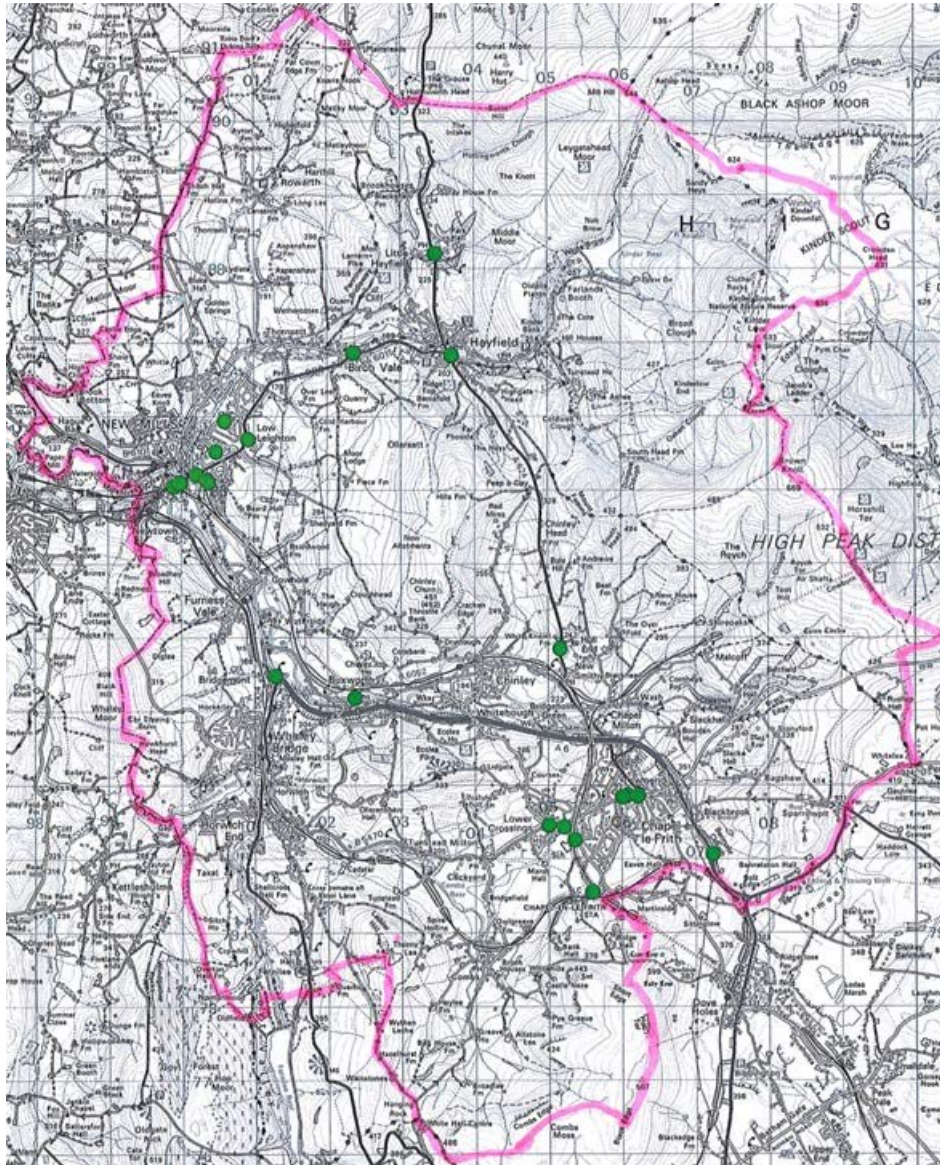
- Pedestrians –particularly 11-14 year olds
- Car passengers aged 5-7 year olds
- Car passengers of all ages travelling with young drivers
- Car passengers travelling into the area from outside the County

Chapel-en-le-Frith, New Mills & District

Casualty numbers in this area were very low

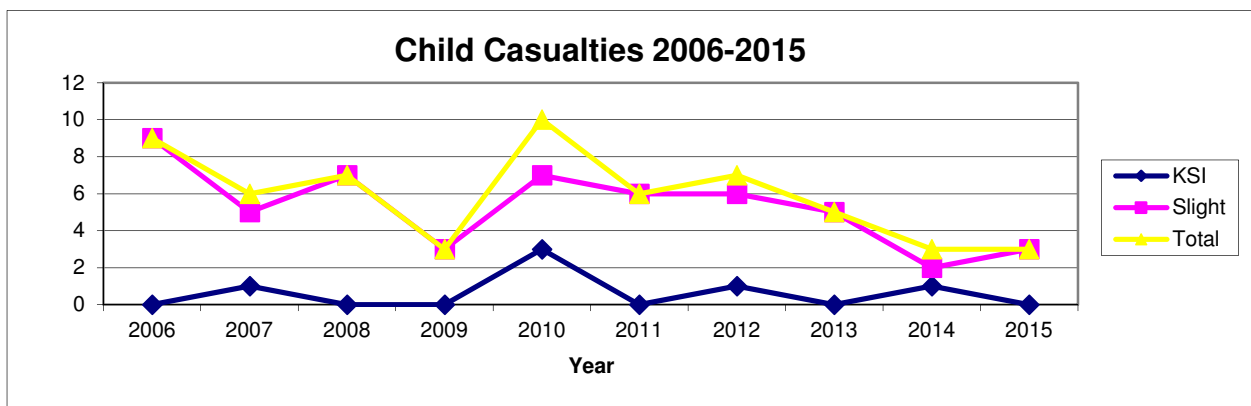
2 secondary schools

14 primary schools



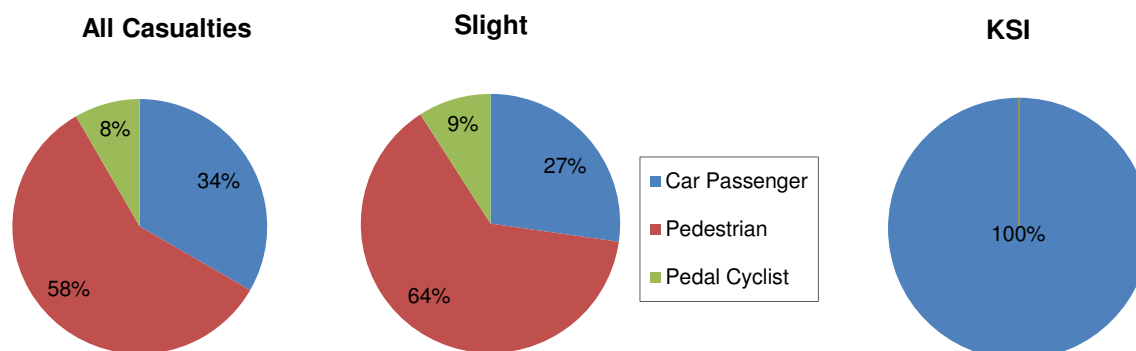
2 KSI casualties 2011-2015

22 Slight casualties 2011-2015



27% decrease in 2011-15 from 2008-2012, better than the County average

	Chapel District	County
Child casualties as a % of all casualties	6%	7%
KSI child casualties as a % of all KSI casualties	4%	6%
Child casualties per 1000 population	4.6	6.8
Pedestrians per 1000 population	2.7	2.3
Pedal Cyclists per 1000 population	0.4	0.9
Car Passengers per 1000 population	1.5	3.1
0-4 year old casualties per 1000	0.0	2.7
5-10 year old casualties per 1000	4.7	6.7
11-15 year old casualties per 1000	8.6	10.3



Notes:

- More pedestrians were injured than the County average
- Less pedal cyclists were injured than the County average
- Less car passengers were injured than the County average
- Only one car passenger was being driven by a young driver
- Nearly three quarters of child casualties were of secondary school age. There were no pre-school aged casualties
- Numbers of casualties aged 14-15 have increased since the 2008-2012 audit. In all other age groups numbers have decreased.
- Nearly three quarters of casualties were boys, a much higher proportion than the County average
- January and March were the months with most casualties
- Nearly 90% of casualties were injured during the week, higher than the County average
- There was a peak in casualties between 3pm and 5pm, over half of all casualties were injured in this two hour period

Where casualty's postcode is recorded:

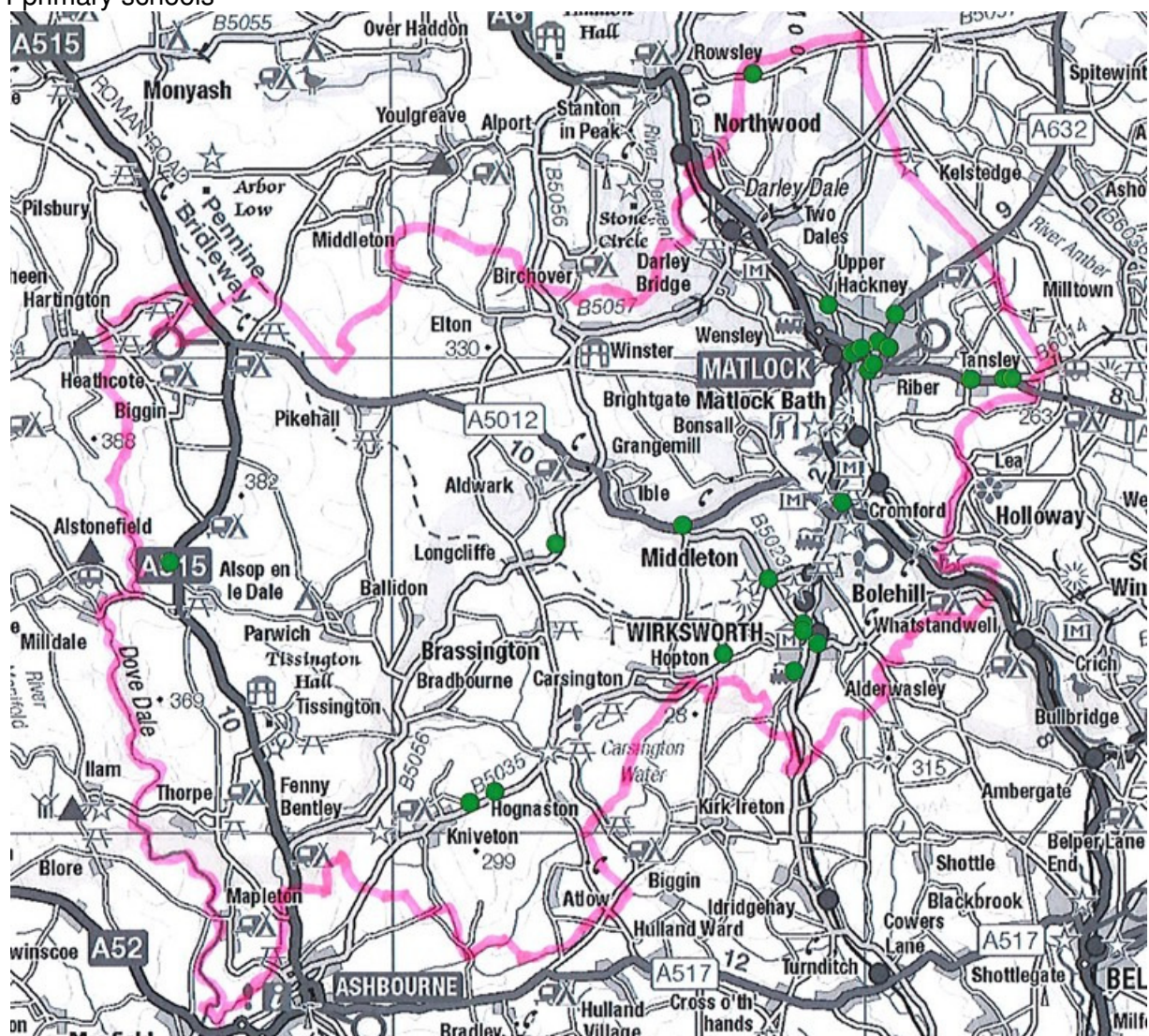
- All pedestrians lived within 4 miles of where they were injured. Nearly two thirds lived within one mile of where they were injured
- 40% of car passengers lived within 2 miles of where they were injured and 40% lived more than 50 miles of where they were injured

Areas to look at:

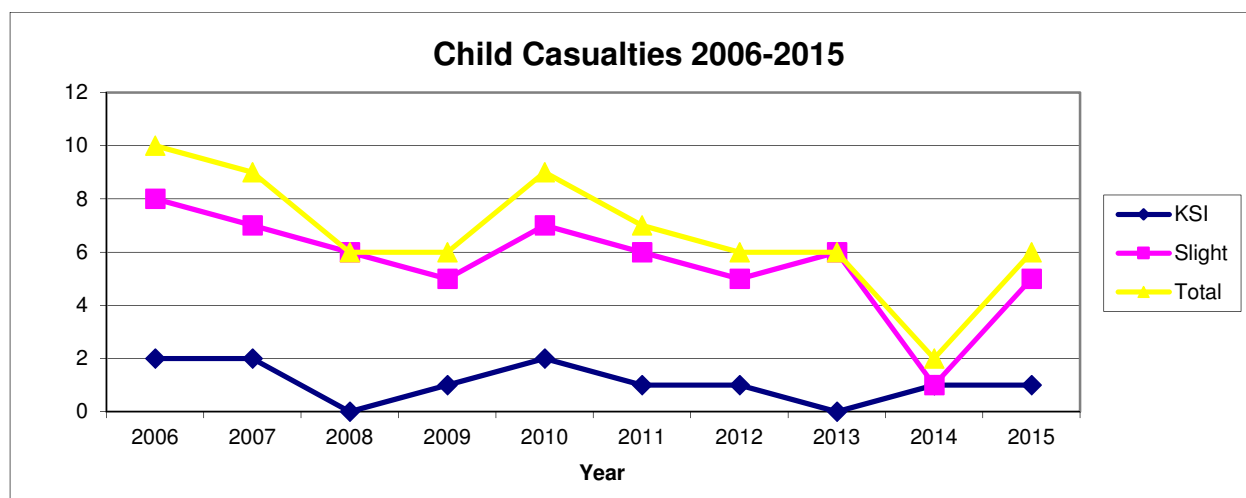
- Pedestrians – particularly 11-14 year olds
- Pedestrians in New Mills
- Car passengers – particularly 13-15 year olds

Matlock, Wirksworth & District

2 secondary schools
24 primary schools

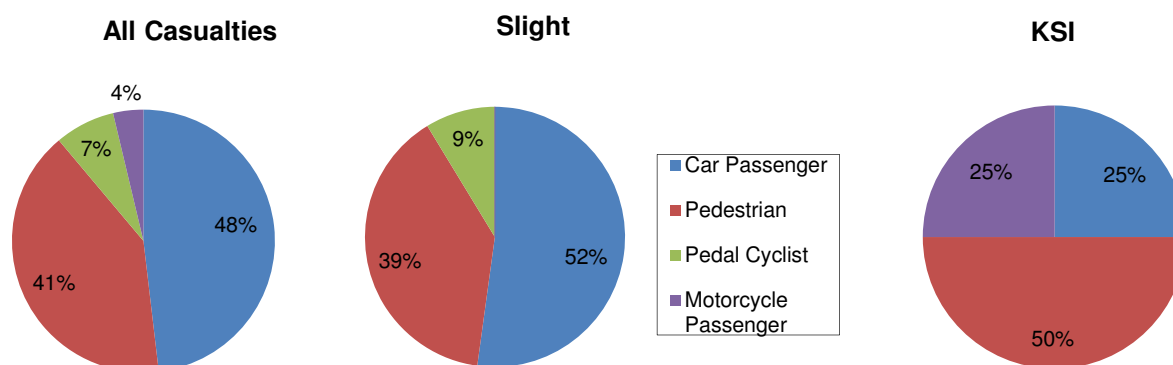


4 KSI casualties 2011-2015
23 Slight casualties 2011-2015



21% decrease in 2011-15 from 2008-2012, similar to the County average

	Matlock District	County
Child casualties as a % of all casualties	5%	7%
KSI child casualties as a % of all KSI casualties	4%	6%
Child casualties per 1000 population	5.5	6.8
Pedestrians per 1000 population	2.2	2.3
Pedal Cyclists per 1000 population	0.4	0.9
Car Passengers per 1000 population	2.6	3.1
0-4 year old casualties per 1000	1.4	2.7
5-10 year old casualties per 1000	6.7	6.7
11-15 year old casualties per 1000	7.6	10.3



Notes:

- A higher proportion of pedestrians were injured compared with the County average
- A lower proportion of pedal cyclists were injured compared with the County average
- Nearly half of child casualties were of primary school age, higher than the figure for the County
- Boys and girls were equally likely to be injured
- August and September were the months with most casualties
- A quarter of casualties were injured at the weekend, similar to the County average
- There were peaks in casualties between noon and 1pm and 4pm and 5pm
- No car passengers were being driven by a young driver

Where casualty's postcode is recorded:

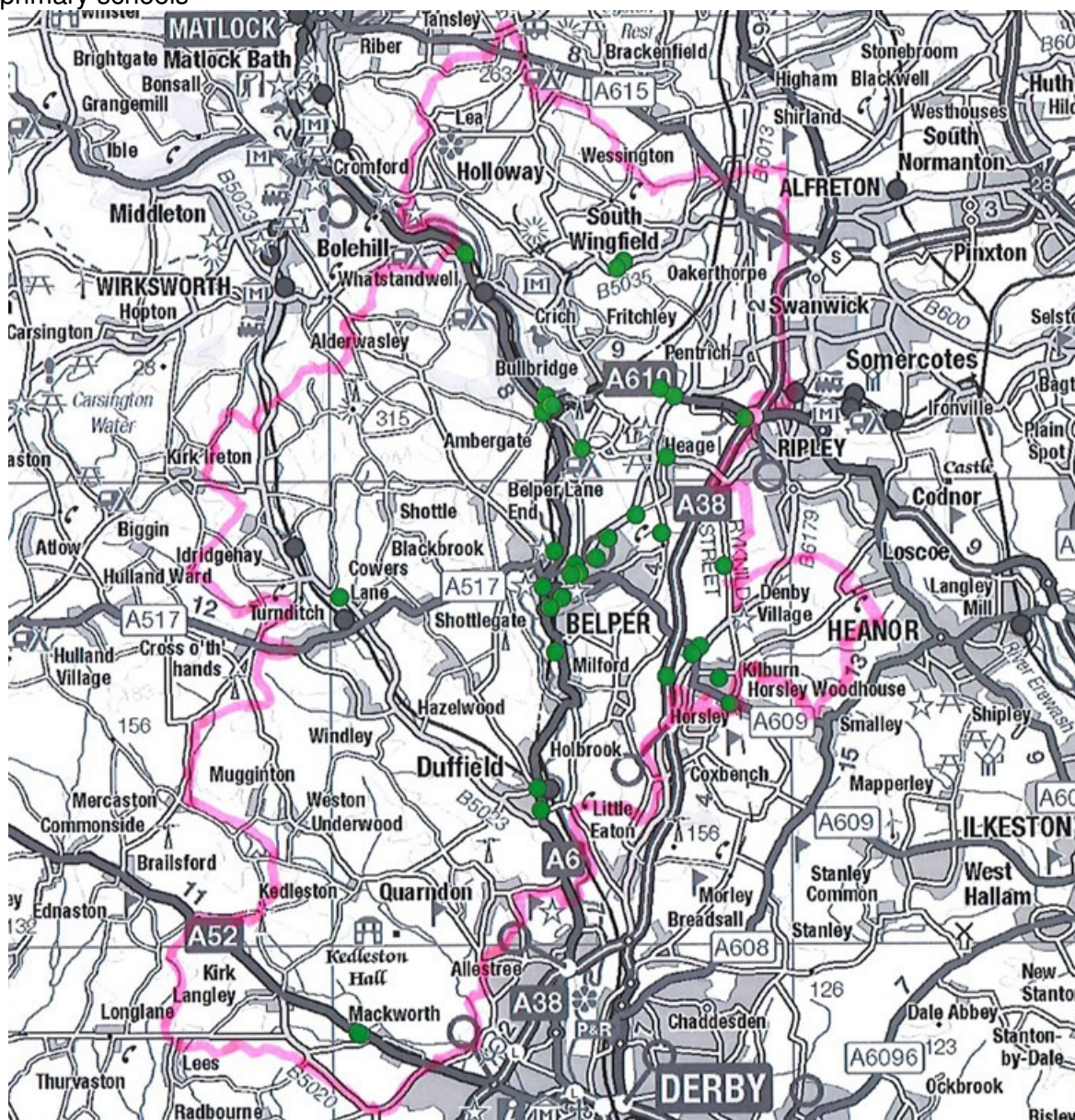
- All but one pedestrian were injured less than a mile from where they lived
- Only one pedal cyclist postcode is recorded. They were injured less than one mile from where they lived
- Just under half of car passengers were injured less than 5 miles from where they lived
- A third of car passengers were injured more than 20 miles from where they lived

Areas to look at:

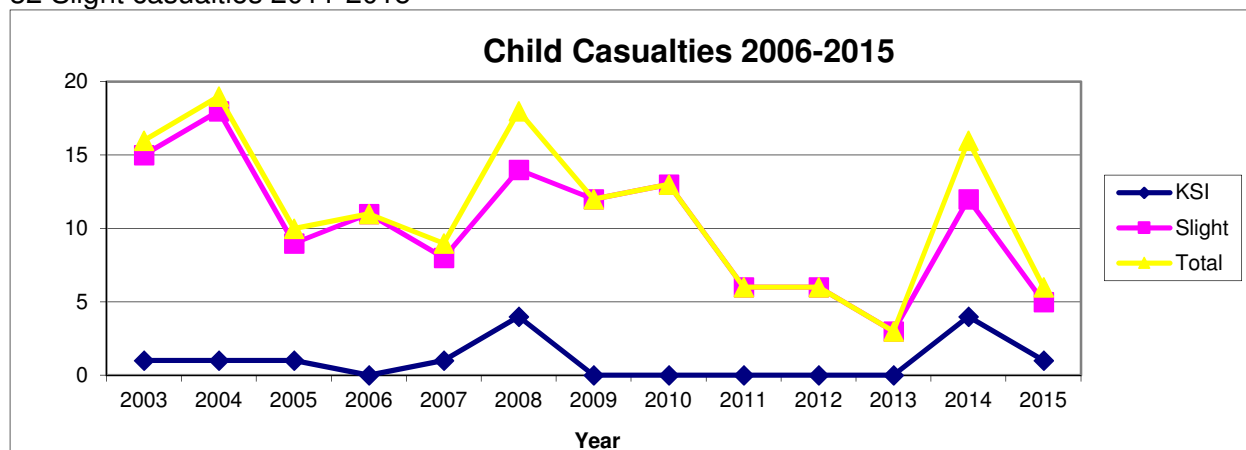
- Pedestrians – 8-9 year olds and of secondary age
- Pedestrians in Matlock
- Car passengers on rural roads

Belper, Duffield & District

3 secondary schools
20 primary schools

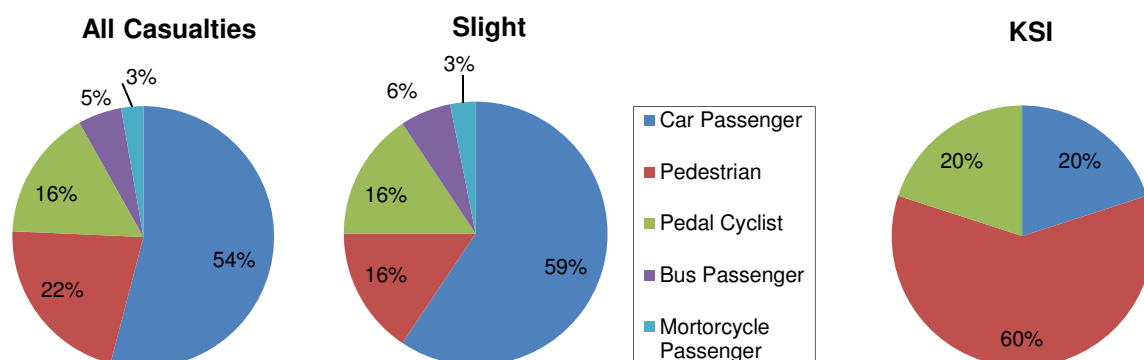


5 KSI casualties 2011-2015
32 Slight casualties 2011-2015



33% decrease in 2011-15 from 2008-2012, better than the County average

	Belper District	County
Child casualties as a % of all casualties	5%	7%
KSI child casualties as a % of all KSI casualties	4%	6%
Child casualties per 1000 population	4.3	6.8
Pedestrians per 1000 population	0.9	2.3
Pedal Cyclists per 1000 population	0.7	0.9
Car Passengers per 1000 population	2.3	3.1
0-4 year old casualties per 1000	1.7	2.7
5-10 year old casualties per 1000	3.8	6.7
11-15 year old casualties per 1000	6.9	10.3



Notes:

- Less pedestrians were injured than the County average
- Slightly more pedal cyclists were injured than the County average
- More car passengers were injured than the County average
- 57% of child casualties were of secondary school age, slightly higher than the figure for the County
- Girls and boys were equally likely to be injured
- January, April and July were the months with most casualties
- Over a third of casualties were injured at the weekend, higher than the County average
- There was a peak in casualties between 4pm and 7pm.
- 21% of car passengers were being driven by a young driver, similar to the County average

Where casualty's postcode is recorded:

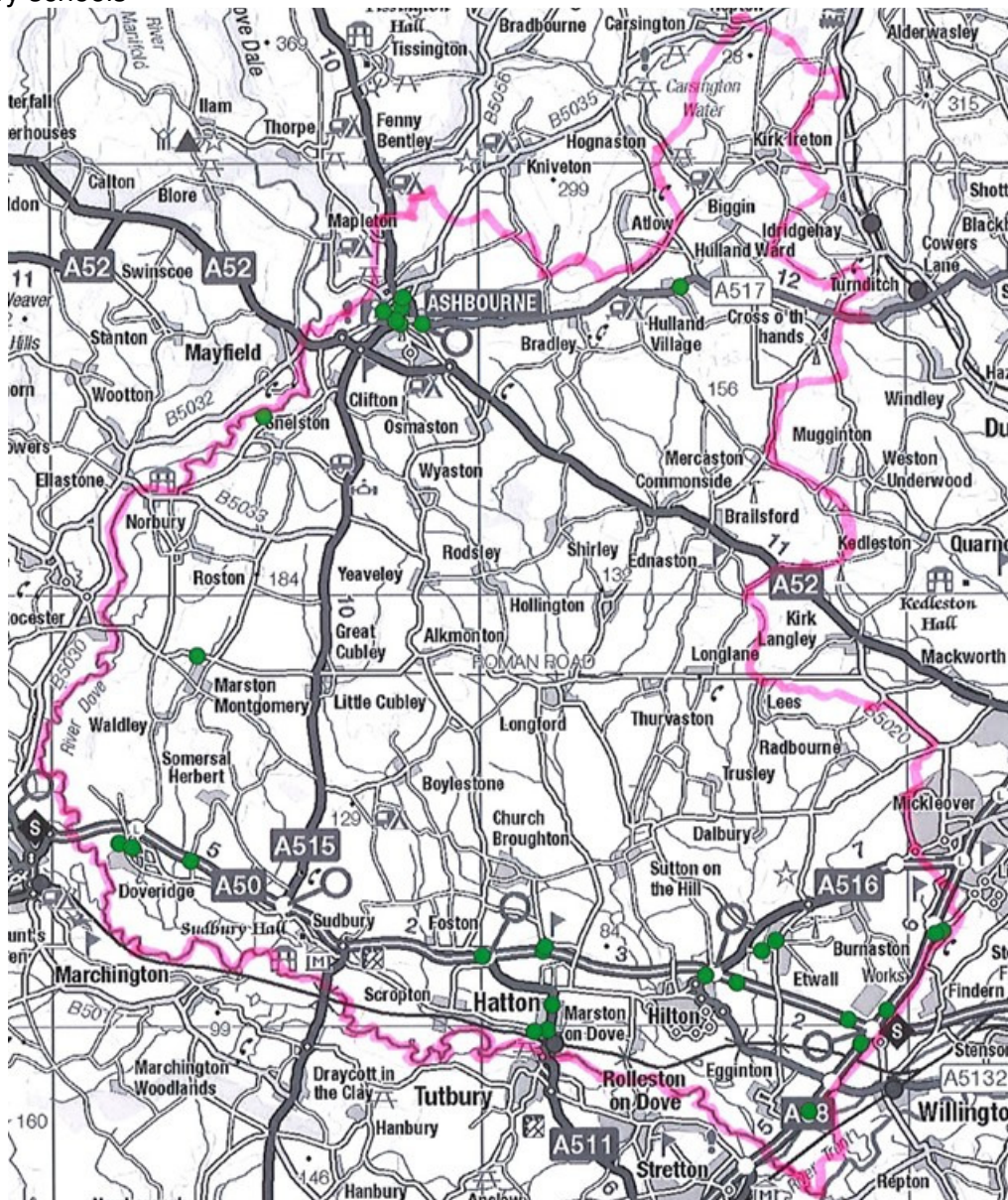
- All pedestrians were injured within 1 mile of where they lived
- Half of pedal cyclists were injured within 1 mile of where they lived
- 60% of car passengers were injured within 10 miles of where they lived

Areas to look at:

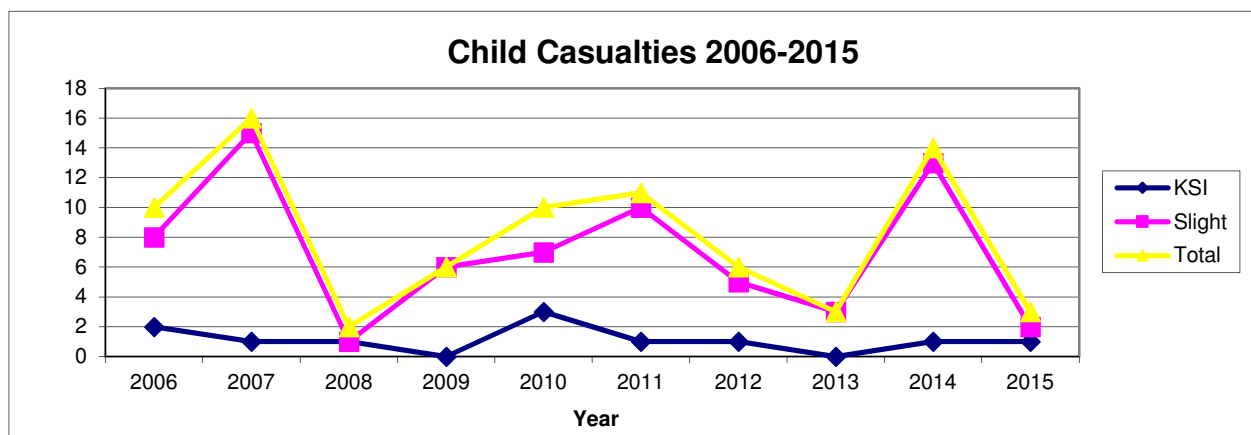
- Car passengers of primary and secondary age
- Pedestrians in Belper

Ashbourne & District

2 secondary schools
20 primary schools

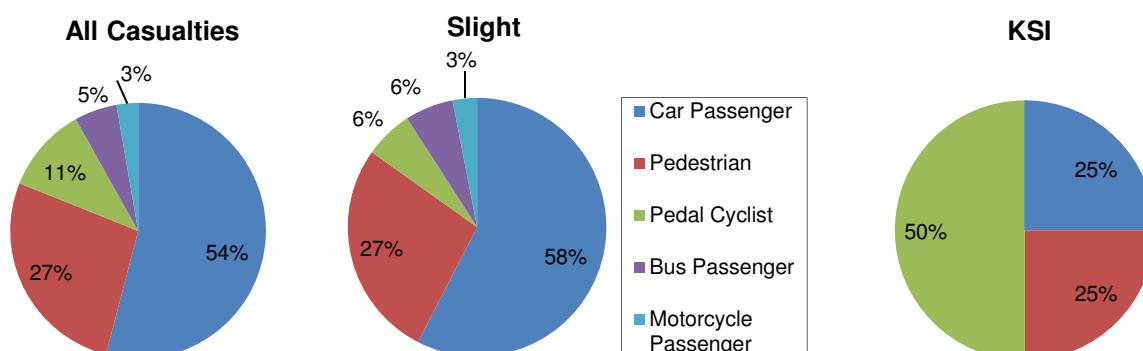


4 KSI casualties 2011-2015
33 Slight casualties 2011-2015



6% increase in 2011-15 from 2008-2012, worse than the County average

	Ashbourne District	County
Child casualties as a % of all casualties	5%	7%
KSI child casualties as a % of all KSI casualties	4%	6%
Child casualties per 1000 population	5.2	6.8
Pedestrians per 1000 population	1.4	2.3
Pedal Cyclists per 1000 population	0.6	0.9
Car Passengers per 1000 population	2.8	3.1
0-4 year old casualties per 1000	2.0	2.7
5-10 year old casualties per 1000	5.0	6.7
11-15 year old casualties per 1000	7.9	10.3



Notes:

- A lower proportion of pedestrians were injured than the County average
- A higher number of car passengers were injured than the County average
- A slightly higher proportion of car passengers were being driven by young drivers than the County average
- A similar proportion of pedal cyclists were injured to the County average
- Half of child casualties were of secondary school age, similar to the figure for the County
- Boys were more likely to be injured than girls
- January, February and November were the months with most casualties
- Nearly three quarters of casualties were injured during the week, similar to the County average
- There were peaks in casualties between 8am and 9 am and 3pm and 7pm
- Nearly half of car passengers were travelling on the A50 or A38 dual carriageways

Where casualty's postcode is recorded:

- All pedestrians lived less than a mile from where they were injured
- All pedal cyclists lived less than a mile from where they were injured
- Half of car passengers lived less than 5 miles from where they were injured
- A quarter of car passengers lived more than 50 miles from where they were injured

Areas to look at:

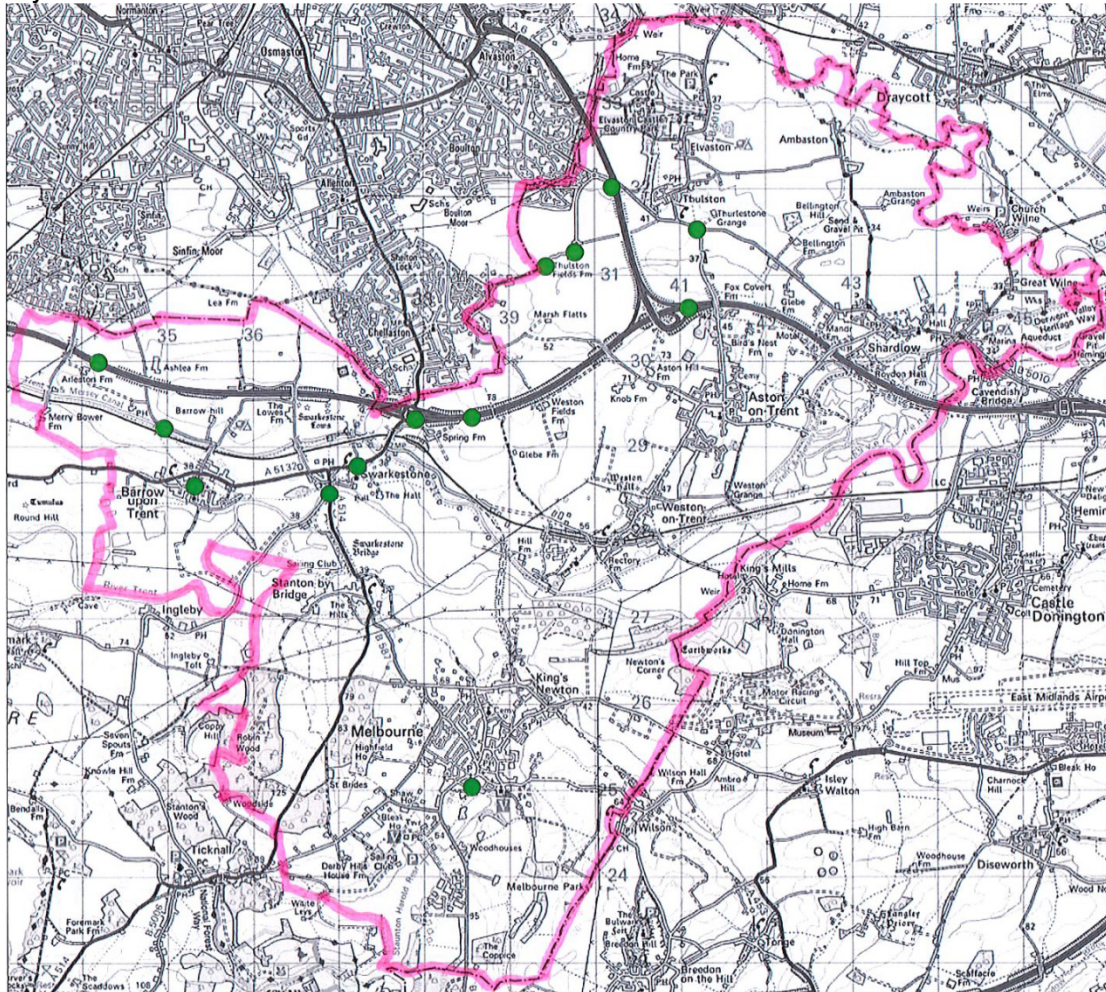
- Car passengers of primary and secondary age
- Pedestrians aged 11-13

Melbourne & District

Casualty numbers in this area were very low

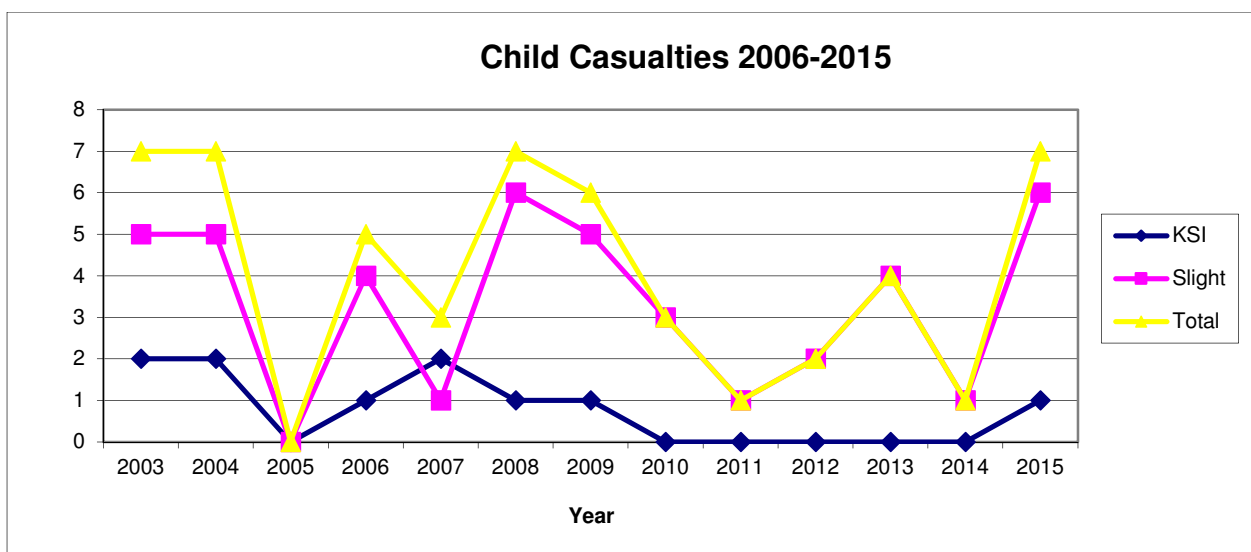
0 secondary schools

6 primary schools



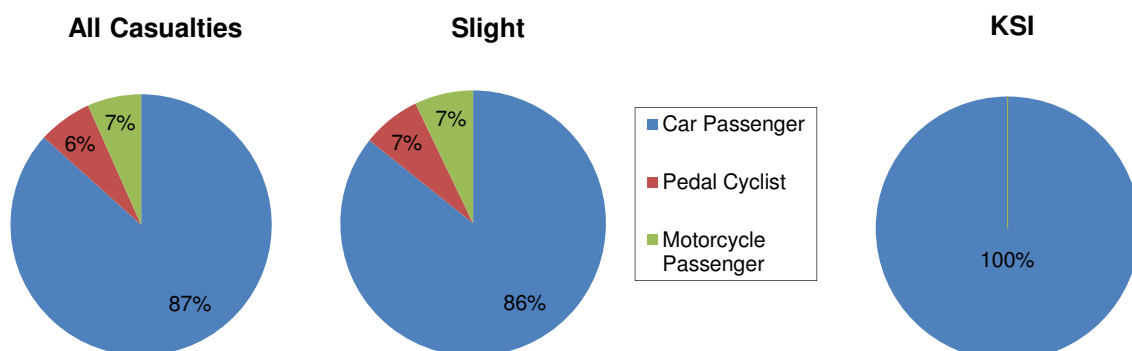
1 KSI casualties 2011-2015

14 Slight casualties 2011-2015



21% decrease in 2011-15 from 2008-2012, similar to the County average

	Melbourne District	County
Child casualties as a % of all casualties	5%	7%
KSI child casualties as a % of all KSI casualties	2%	6%
Child casualties per 1000 population	6.8	6.8
Pedestrians per 1000 population	0.0	2.3
Pedal Cyclists per 1000 population	0.5	0.9
Car Passengers per 1000 population	5.9	3.1
0-4 year old casualties per 1000	9.2	2.7
5-10 year old casualties per 1000	2.5	6.7
11-15 year old casualties per 1000	9.4	10.3



Notes:

- A much higher proportion of car passengers were injured than the County average
- Only one pedal cyclist and no pedestrians were injured in this area
- 40% of child casualties were of pre-school age, a lot higher than in the County
- More girls than boys were injured
- July was the month with most casualties
- Most casualties were injured during the week
- There was a peak in casualties between and 2pm and 4pm
- No car passengers were being driven by young drivers

Where casualty's postcode is recorded:

- The one pedal cyclist was injured less than half a mile from where they lived
- Three quarters of car passengers were injured less than 5 miles from where they lived
- Two car passengers were injured more than 30 miles from where they lived

Areas to look at:

- Car passengers – particularly 0-4 year olds and 11-13 year olds